

Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

SULLIVAN COUNTY

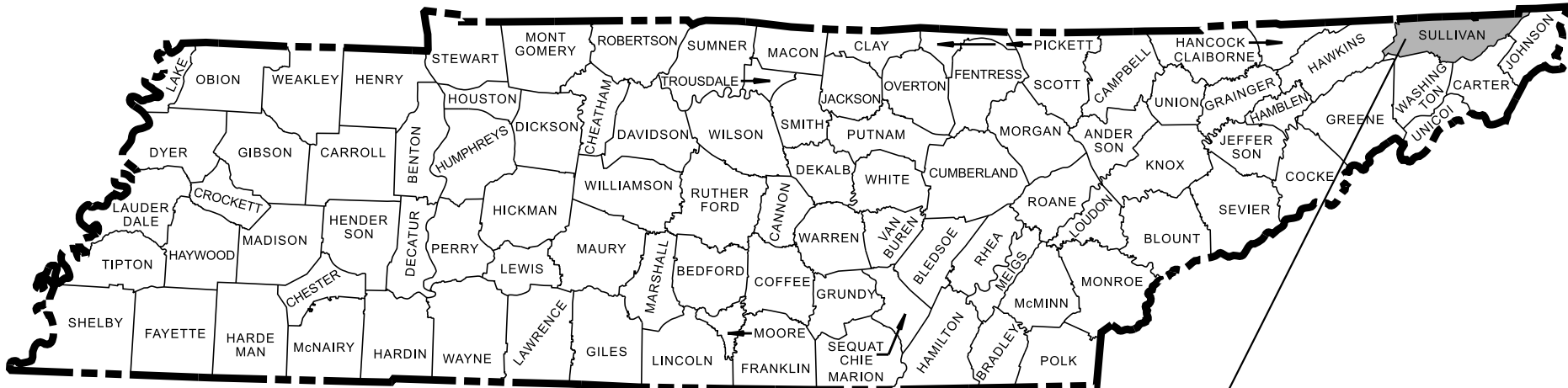
I-81:
FROM NEAR I-26
TO NEAR SR-357

RESURFACE
MILL, CM, OGFC, GUARDRAIL, AND PAVEMENT MARKINGS

STATE HIGHWAY NO. N/A F.A.H.S. NO. I-81

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES X	NO

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	NH-I-81-1(142)	
STATE PROJ. NO.	82I081-F8-007	



SULLIVAN COUNTY I-81

BRIDGE ID. # 82I00810011 82I00810012 82I00810013
82I00810014 82I00810015 82I00810016
82I00810017 82I00810018 82I00810019
82I00810020

NO EXCLUSIONS

82I081-F8-007
END PROJECT NO. NH-I-81-1(142) RESURFACE
L.M. 9.38

CSX TRANSPORTATION (CSXT)
R/R OVERPASS CROSSING # 243997L
LAT 36.4735850 LONG -82.4933690
MP Z-102.15 L.M. 5.41

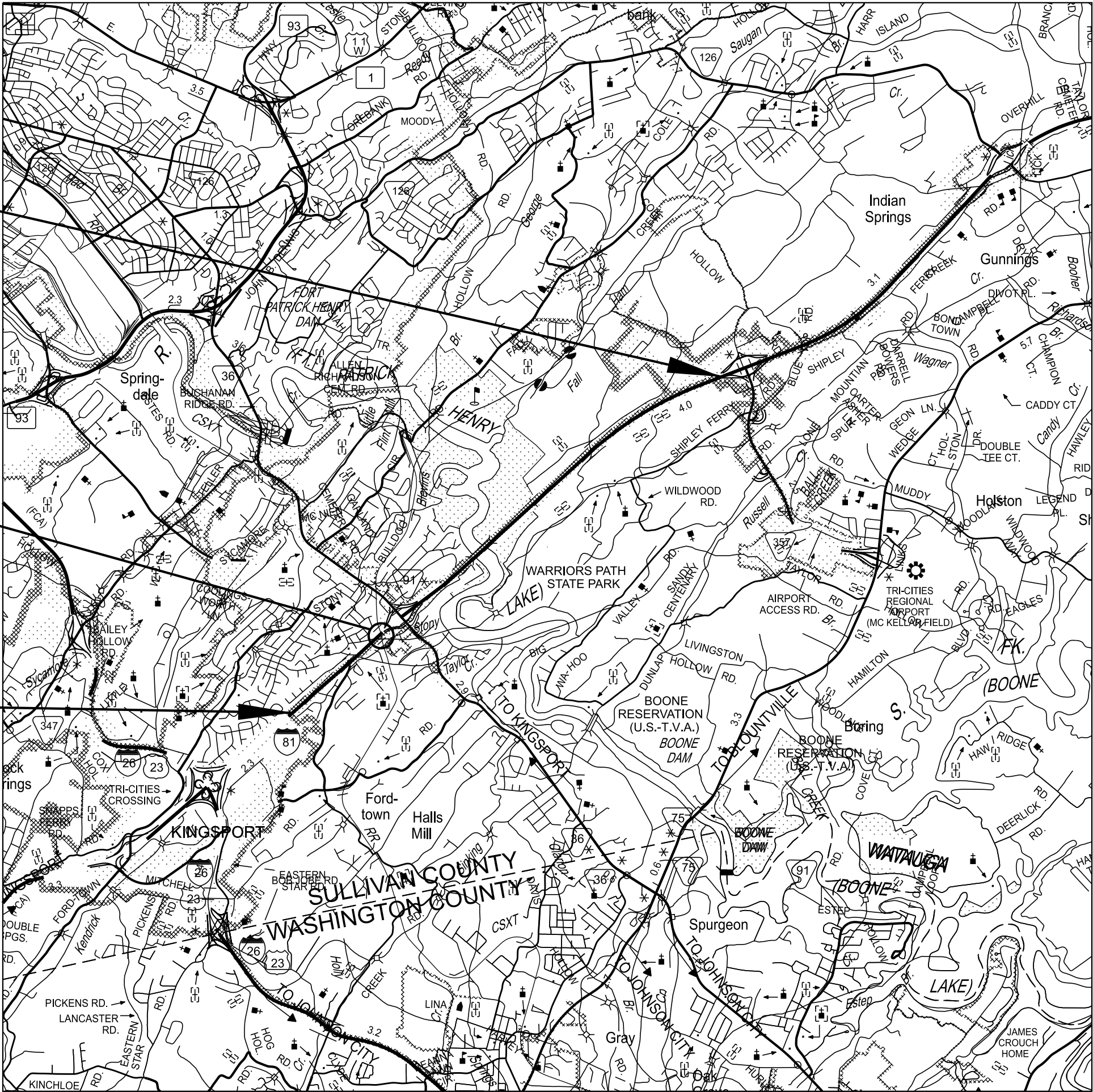
82I081-F8-007
BEGIN PROJECT NO. NH-I-81-1(142) RESURFACE
L.M. 4.31

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : ERIC WILSON, P.E.
DESIGN FIRM : ARCADIS US, INC.
DESIGNER : ADAM GRAVITT CHECKED BY ANDREW PARR, P.E.
P.E. NO. 98012-4199-04 (DESIGN)
PIN NO. 129683.00



SCALE: 1"= 5280' 0 1 2 3 MILES

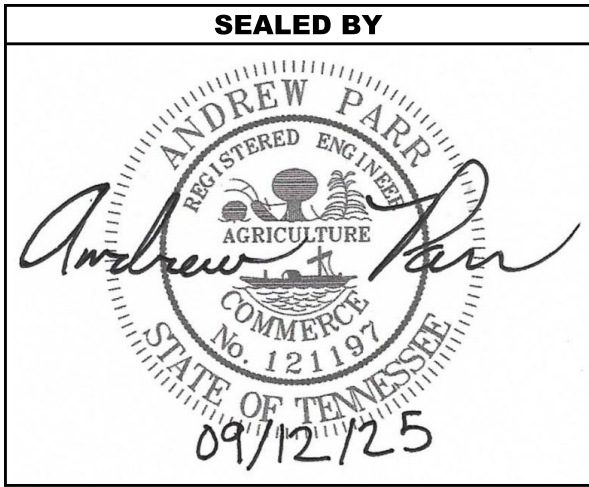
PROJECT LENGTH 5.07 MILES
TOTAL LANE MILES RESURFACED 22.92 MILES

TRAFFIC COUNTER & WEATHER STATIONS

STATION LOCATION	L.M. 5.10
STATION LOCATION	L.M. 8.15

TRAFFIC DATA

ADT (2025)	32692
POSTED SPEED	70 MPH



APPROVED: SHANE HESTER, CHIEF ENGINEER

DATE:

APPROVED: WILL REID, COMMISSIONER

ROADWAY INDEX

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS, AND	
STANDARD TRAFFIC DESIGN DRAWINGS	1A
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B, 2B1
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SPECIAL NOTES	2D, 2D1
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EROSION PREVENTION AND SEDIMENT CONTROL NOTES	2E1
TABULATED QUANTITIES	2F
DETAIL SHEET	2G
UTILITY NOTES AND UTILITY OWNERS	3
RAMP DETAILS	4,4A,4B
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	T1
NO UTILITY SHEETS ARE INCLUDED IN THIS SET OF PLANS	
NO PROJECT COMMITMENT SHEETS ARE INCLUDED IN THIS SET OF PLANS	

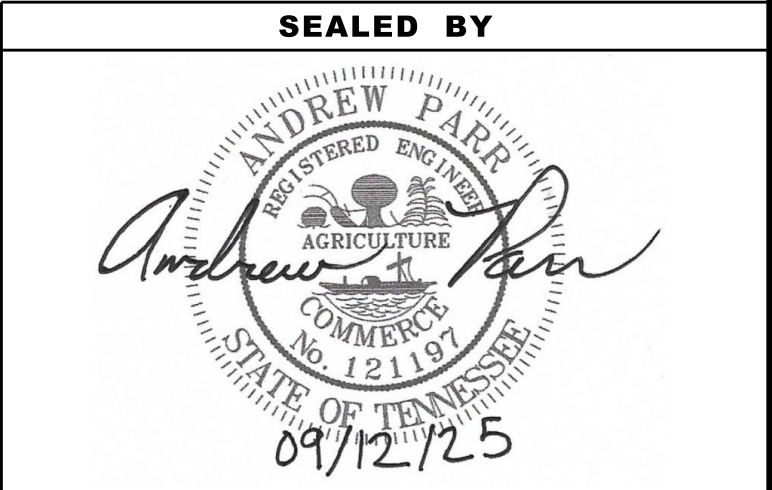
STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
ROADWAY DESIGN STANDARDS		
RD11-SE-1		TRANSITION AND CROSS SLOPE DETAILS
RD11-SE-3		SUPERELEVATION TRANSITION DETAILS FOR DIVIDED ROADWAYS
RD11-SE-3A		SUPERELEVATION TRANSITION SECTIONS FOR DIVIDED ROADWAYS
RD11-LR-1		MINIMUM RUNOFF LENGTHS (LR) FOR URBAN HIGHWAYS
RD11-LR-2		MINIMUM RUNOFF LENGTHS (LR) FOR RURAL HIGHWAYS
SAFETY DESIGN AND GUARDRAILS		
S-CZ-1	06-28-19	CLEAR ZONE CRITERIA
S-PL-1	03-01-23	SAFETY PLAN FOR BARRIER LENGTH OF NEED
S-PL-1A	03-01-23	SAFETY PLAN FOR BARRIER LENGTH OF NEED (FOR RIGID OBJECTS)
S-PL-1B	03-01-23	SAFETY PLAN FOR BARRIER LENGTH OF NEED ON CURVED ROADWAYS
S-PL-6	07-30-24	SAFETY PLAN SAFETY HARDWARE PLACEMENT ON OUTSIDE EDGE
S-PL-6A	06-28-19	SAFETY PLAN SAFETY HARDWARE PLACEMENT IN MEDIAN
S-GR31-1	03-13-25	GUARDRAIL DETAILS
S-GRC-4	01-30-25	GUARDRAIL CONNECTION TO BRIDGE RAILING CONCRETE PARAPET
S-GRC-5	02-28-20	GUARDRAIL CONNECTION TO BRIDGE ENDS (TRAILING ENDS)
S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2P	10-16-20	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-2	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS
T-M-5	01-24-25	MARKING DETAIL FOR FREEWAYS
T-M-6	01-24-25	MARKING DETAIL FOR EXPRESSWAY AND FREEWAY INTERCHANGES
T-M-7	01-24-25	GORE MARKING DETAILS FOR EXPRESSWAY & FREEWAY INTERCHANGES
T-M-8	01-24-25	MARKING DETAILS FOR EXPRESSWAYS & FREEWAYS
T-M-9	01-24-25	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9A	01-24-25	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-9B	07-22-25	PAVEMENT MARKING AND SIGNING DETAILS FOR RAMP INTERSECTIONS
T-M-15	01-24-25	ASPHALT SHOULDER RUMBLE STRIP INSTALLATION DETAILS FOR INTERSTATE AND ACCESS CONTROLLED ROUTES
T-WZ-10	03-26-25	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS
T-WZ-11	03-26-25	ONE LANE CLOSURE DETAIL ON DIVIDED HIGHWAYS
T-WZ-12	03-26-25	ONE LANE CLOSURE DETAIL FOR BRIDGES ON DIVIDED HIGHWAYS
T-WZ-18	03-26-25	SHOULDER CLOSURE DETAIL FOR FREEWAYS AND DIVIDED HIGHWAYS
T-WZ-60	03-26-25	FREEWAY RESURFACING SIGNING PROCEDURES
T-WZ-63	03-26-25	WORK ZONE IN THE VICINITY OF AN ENTRANCE RAMP
T-WZ-64	03-26-25	WORK ZONE IN THE VICINITY OF AN EXIT
T-WZ-FAB1	03-26-25	FLASHING YELLOW ARROW BOARD

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-81-1(142)	1A



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

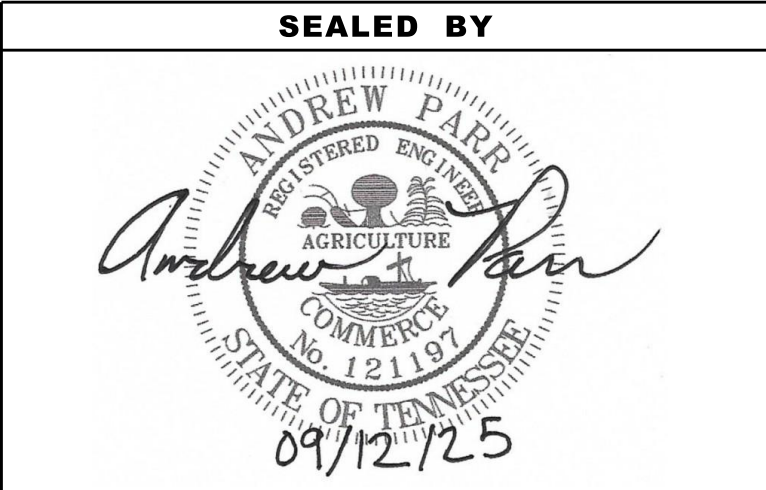
ROADWAY INDEX,
STANDARD ROADWAY
DRAWINGS, AND
STANDARD TRAFFIC
DESIGN DRAWINGS

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-81-1(142)	2

ESTIMATED ROADWAY QUANTITIES			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY 821081-F8-007
(1)	201-01 CLEARING AND GRUBBING	LS	1
(2)	208-01.05 BROOMING & DEGRASSING SHOULDERS	L.M.	21
(3)	303-01 MINERAL AGGREGATE, TYPE A BASE, GRADING D	TON	269
(3)	307-03.12 ASPHALT CONCRETE MIX (PG76-22) GRADING CM	TON	11331
(3)	403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)	TON	74
(3)(4)	411-03.10 ACS MIX(PG76-22) GRADING D	TON	3600
(3)(5)	411-03.23 ACS MIX (PG76-22) OGFC	TON	12350
	411-12.01 SCORING SHOULDERS (CONTINUOUS) (16IN WIDTH)	L.M.	21
(3)	415-01.01 COLD PLANING BITUMINOUS PAVEMENT	TON	15760
(6)	502-08.01 RESEALING JOINTS (HOT POURED ELASTIC)	L.F.	6250
(3)	705-02.10 GUARDRAIL TRANSITION 27IN TO 31IN	EACH	6
(3)(7)	705-04.09 EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH	1
(3)	705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH	6
(8)	706-01 GUARDRAIL REMOVED	L.F.	282
	712-01 TRAFFIC CONTROL	LS	1
	712-04.01 FLEXIBLE DRUMS (CHANNELIZING)	EACH	261
	712-05.01 WARNING LIGHTS (TYPE A)	EACH	131
(9)	712-06 SIGNS (CONSTRUCTION)	S.F.	1767
(10)	712-06.16 SIGNS (CONSTRUCTION)(REDUCED SPEED WARNING)	EACH	4
	712-08.03 ARROW BOARD (TYPE C)	EACH	2
	712-08.08 SPEED FEEDBACK SIGN ASSEMBLY	EACH	4
	712-08.09 DIGITAL SPEED LIMIT SIGN ASSEMBLY	EACH	8
	712-08.12 QUEUE PROTECTION TRUCK	DAY	42
	713-16.01 CHANGEABLE MESSAGE SIGN UNIT	EACH	2
(11)	716-01.23 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR)(2 COLOR)	EACH	919
(12)	716-01.30 REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	919
(13)	716-02.07 PLASTIC PAVEMENT MARKING (24" BARRIER LINE)	L.F.	415
(13)	716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	60
(13)	716-04.06 PLASTIC PAVEMENT MARKING (WRONG WAY ARROW)	EACH	2
(13)	716-04.14 PLASTIC PAVEMENT MARKING (LANE REDUCTION ARROW)	EACH	3
(14)	716-05.20 PAINTED PAVEMENT MARKING (6" LINE)	L.M.	25
(15)	716-12.02 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.	25
(15)	716-12.03 ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE)	L.F.	1588
(15)	716-12.05 ENHANCED FLATLINE THERMO PVMT MRKNG (6IN DOTTED LINE)	L.F.	1906
	717-01 MOBILIZATION	LS	1
	ALTERNATE AA1		
(3)(16)	403-02.01 TRACKLESS TACK COAT	TON	226
	ALTERNATE AA2		
(3)(17)	403-02.02 HOT APPLIED TACK COAT	TON	170

FOOTNOTES	
(1)	SEE DETAIL ON SHEET 2G.
(2)	INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET NO. 2C, FINAL PAVEMENT MARKING NOTE (6) FOR MORE INFORMATION.
(3)	SEE SHEET 2F FOR MORE INFO.
(4)	TO BE USED ON ALL BRIDGES WITHIN PROJECT LIMITS.
(5)	<u>230 TONS</u> TO BE USED FOR PLANT STARTUP AND HEATING UP EQUIPMENT AT BEGINNING OF EACH SHIFT, <u>15 TONS</u> WASTE MATERIAL PER DAY.
(6)	TO BE USED TO SEAL SHOULDER JOINTS ON CONCRETE RAMPS.
(7)	INCLUDES ALL MATERIAL NECESSARY FOR THE CONSTRUCTION OF THE PAD AND ANY NECESSARY EPSC MATERIALS.
(8)	GUARDRAIL SHALL BE REMOVED SUCH THAT NEW COMBINED GUARDRAIL AND END TERMINAL LENGTH IS EQUAL TO OR GREATER THAN EXISTING.
(9)	SEE SHEET 2F FOR SIGN TABULATED QUANTITIES. THE CONSTRUCTION SIGNING IS TO BE A MINIMUM. OTHER SIGNS MAY BE REQUIRED AS DIRECTED BY THE TDOT MANAGER.
(10)	ITEM TO BE USED ONLY WHEN A REDUCED SPEED LIMIT IS ESTABLISHED WITHIN THE PROJECT CONSTRUCTION WORK ZONE LIMITS. ITEM INCLUDES SIGN FACE, SUPPORTS AND TWO (2) TYPE "B" FLASHERS PER STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TURNING ON THE TYPE "B" FLASHERS WHEN WORKERS ARE IN THE CONSTRUCTION WORK ZONE AND TURNING THEM OFF WHEN WORKERS ARE NO LONGER IN THE CONSTRUCTION WORK ZONE.
(11)	INCLUDES 26 (EA) FOR WRONG-WAY ARROWS.
(12)	ANY DAMAGE THAT OCCURS DURING REMOVAL SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE TDOT MANAGER.
(13)	THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.
(14)	TO BE USED FOR TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS.
(15)	CONTRACTOR SHALL USE THE EXTRUDED OR RIBBON METHOD FOR APPLICATION
(16)	USE AN APPROVED TRACKLESS TACK FROM QPL 40-F APPLIED WITH A DISTRIBUTOR AT A MINIMUM RATE OF 0.25 GAL/SY (APPROXIMATE RESIDUAL RATE 0.13-0.15 GAL/SY). PAVING SHALL NOT BEGIN UNTIL THE DEPARTMENT IS SATISFIED THE APPLICATION RATE IS ACHIEVED AND THE EMULSION HAS FULLY BROKEN. MULTIPLE PASSES MAY BE REQUIRED. OR, EMULSION TYPE CQS-1HP MAY BE APPLIED WITH A SPRAY PAVER AT AN APPLICATION RATE BETWEEN 0.18 TO 0.23 GAL/SY. THE SPRAY PAVER SHALL BE A SINGLE PIECE OF EQUIPMENT THAT APPLIES THE TACK COAT AND SPREADS THE BITUMINOUS PAVEMENT. AT A MINIMUM THE SPRAY PAVER SHALL MEET THE PAVER REQUIREMENTS OF 407.06 AND THE DISTRIBUTOR REQUIREMENTS IN 402.03.
(17)	HOT APPLIED TRACKLESS TACK MAY BE EITHER HOT APPLIED ASPHALT BINDER OR AN APPROVED HOT APPLIED TRACKLESS TACK COAT FROM QPL 40, SECTION F. IF USING ASPHALT BINDER THE MINIMUM GRADE SHALL BE PG64-22 BUT A HIGHER GRADE MAY BE USED.

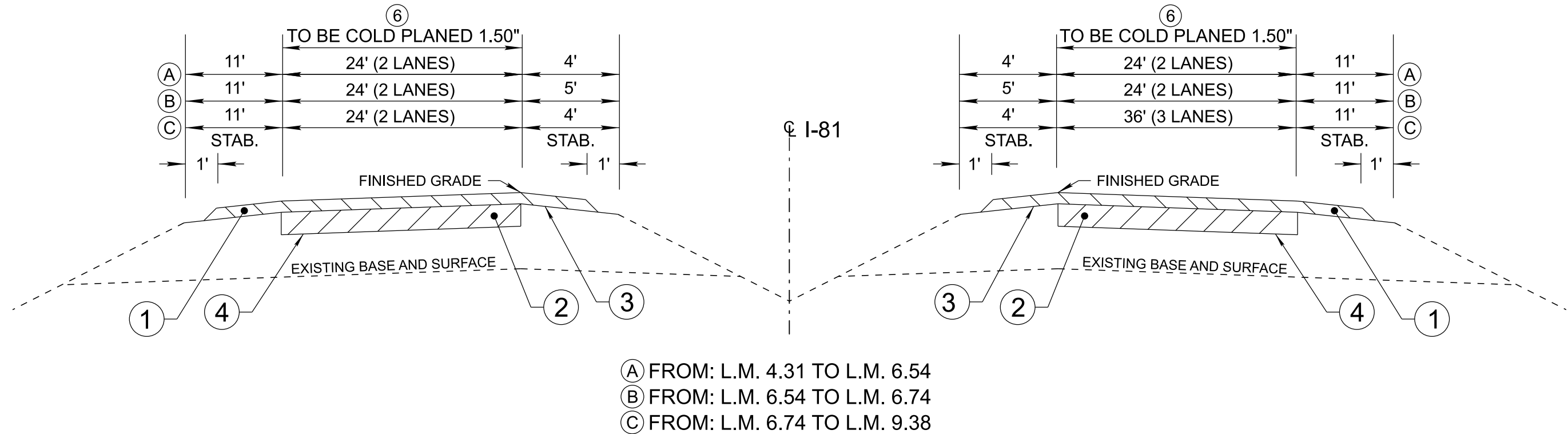
NOTE: THERE ARE NO UTILITY ADJUSTMENTS WITHIN PROJECT LIMITS



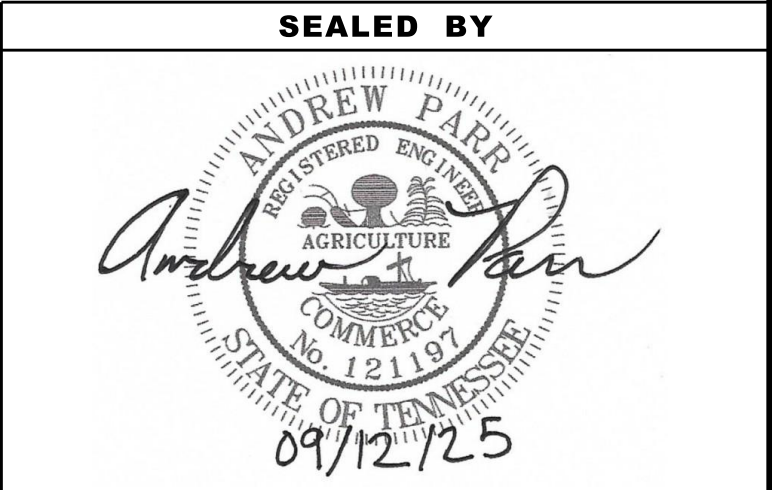
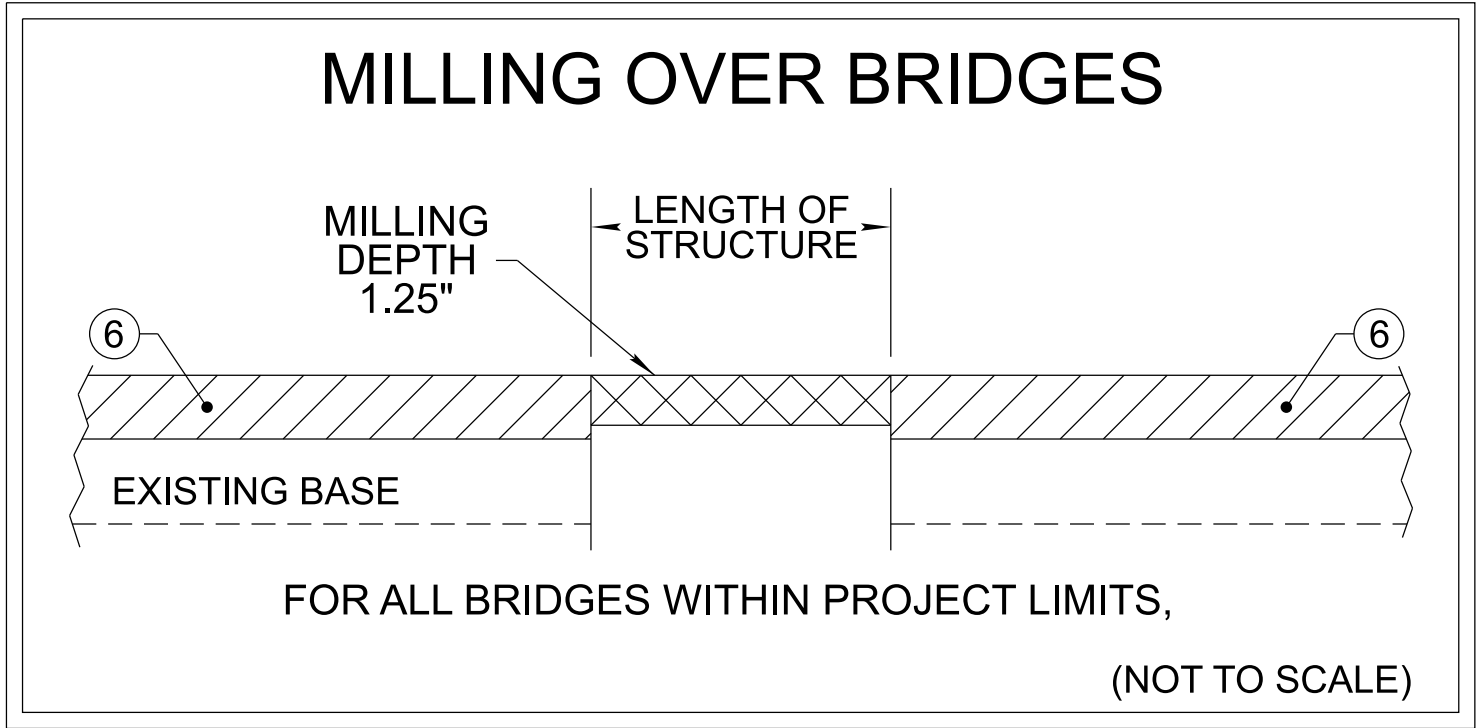
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED
ROADWAY
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-81-1(142)	2B



PROPOSED PAVEMENT SCHEDULE	
① ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 117.5 LBS./S.Y.) ITEM 411-03.23 ACS MIX (PG76-22) OGFC	④ TACK COAT ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD
② BITUMINOUS COURSE @ 1.50"± THICK (APPROX. 165.0 LBS./S.Y.) ITEM 307-03.12 ASPHALT CONCRETE MIX (PG76-22) GRADING "CM"	⑥ COLD PLANING @ 1.50"± THICK (APPROX. 157.5 LBS./S.Y.) ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT
ALTERNATE AA1	ALTERNATE AA2
③ TRACKLESS TACK COAT ITEM 403-02.01 TRACKLESS TACK COAT (TC) SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD	③ TACK COAT (TC) (APPROX. 0.15 GAL./S.Y.) ITEM 403-02.02 HOT APPLIED TACK COAT

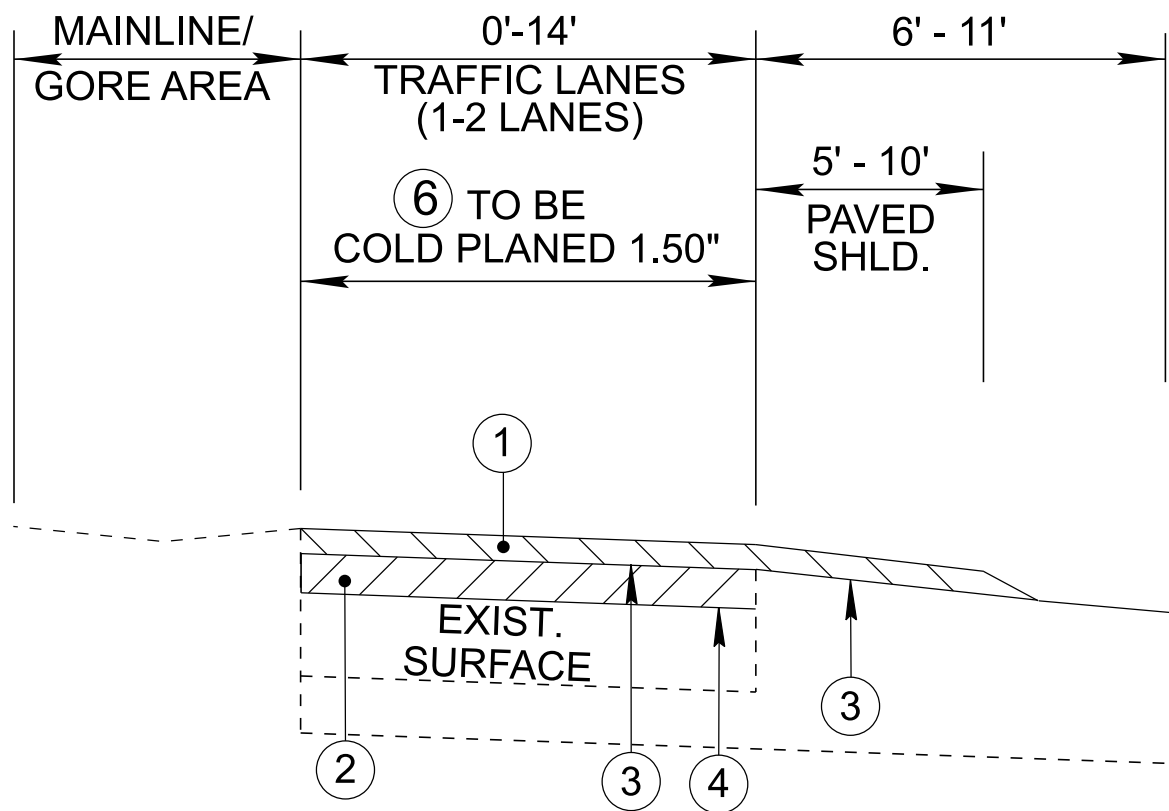


STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

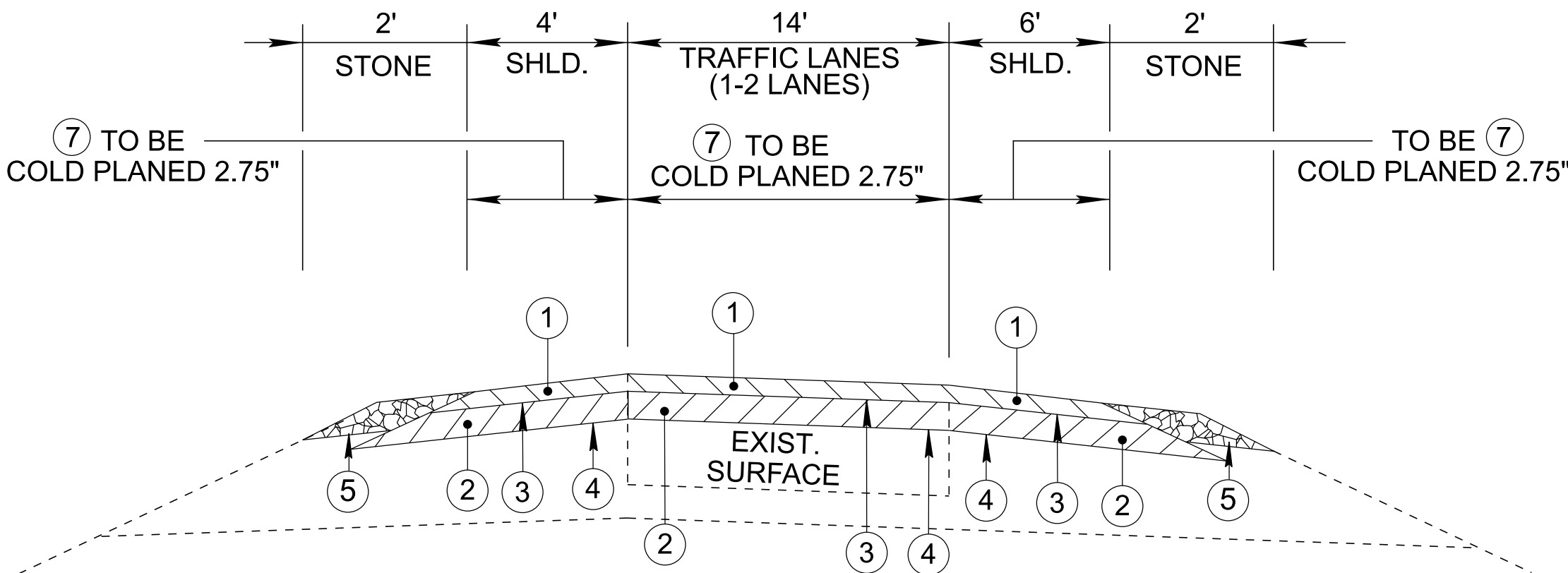
TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-81-1(142)	2B1

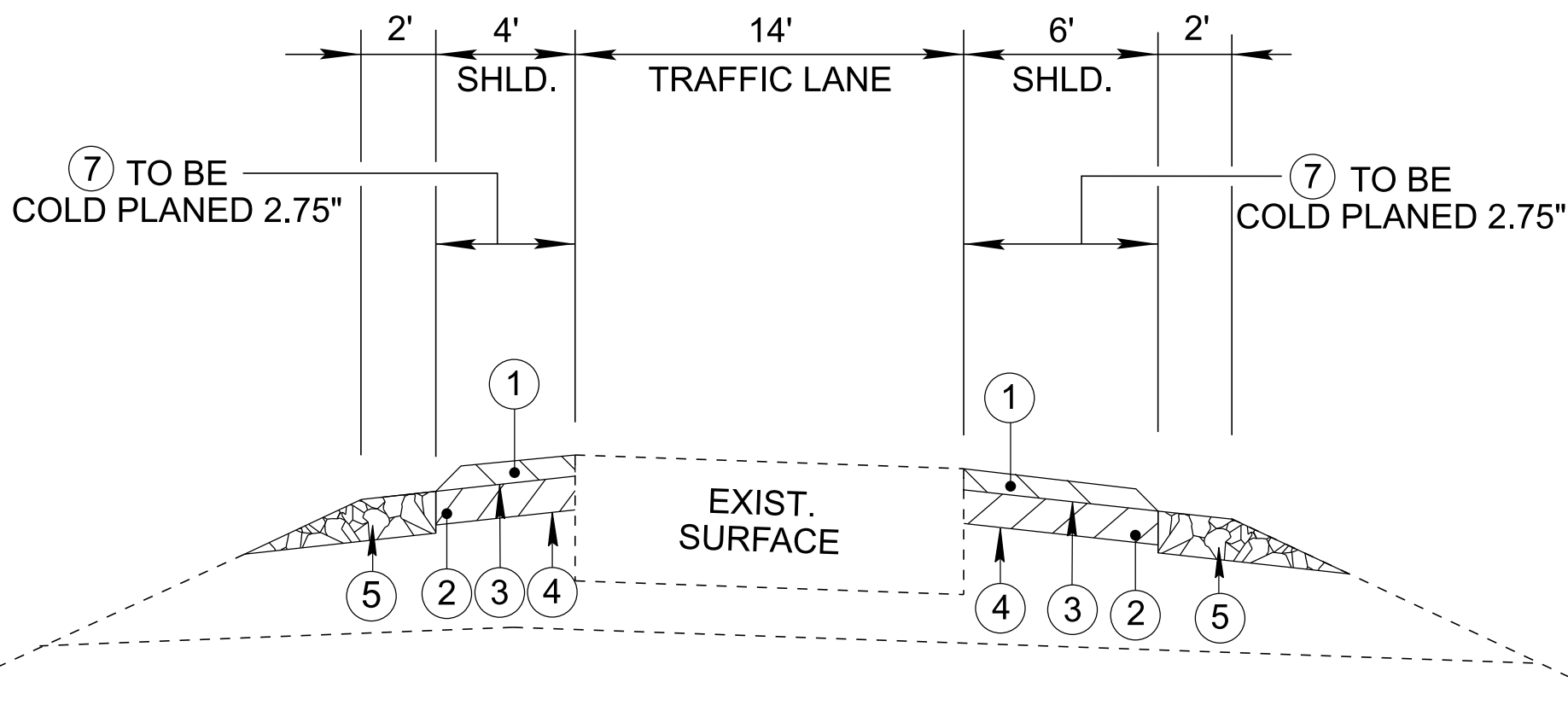
ASPHALT TYPICAL 1



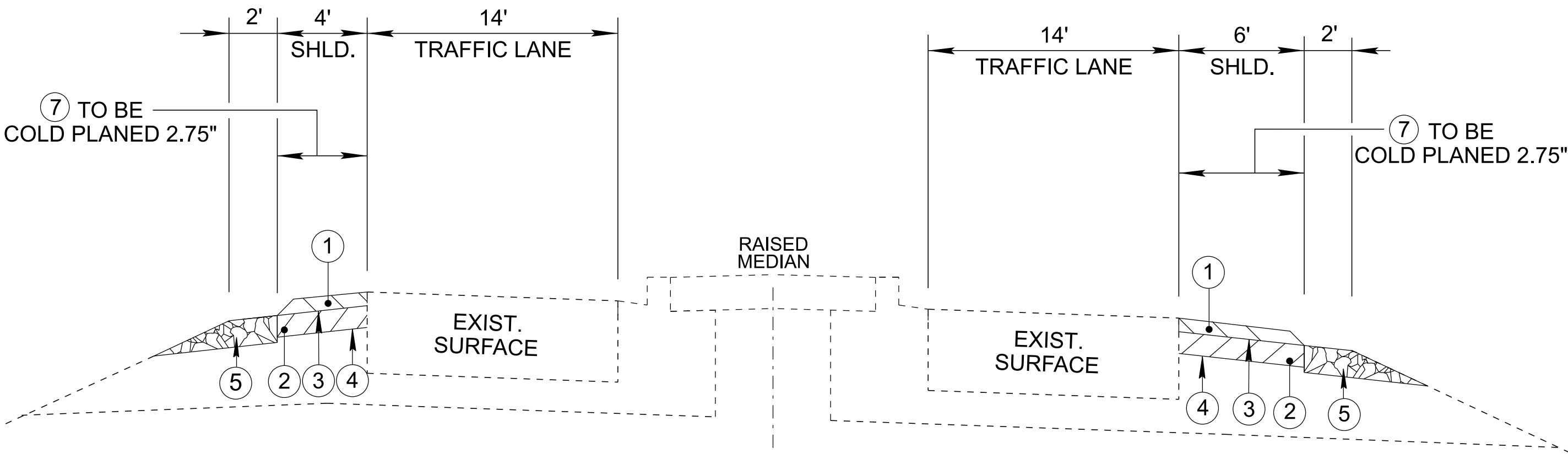
ASPHALT TYPICAL 2



CONCRETE TYPICAL 1



CONCRETE TYPICAL 2



PROPOSED PAVEMENT SCHEDULE

① ASPHALTIC CONCRETE SURFACE (HM) @ 1.25"± THICK (APPROX. 117.5 LBS./S.Y.)
ITEM 411-03.23 ACS MIX (PG76-22) OGFC

② BITUMINOUS COURSE @ 1.50"± THICK (APPROX. 165.0 LBS./S.Y.)
ITEM 307-03.12 ASPHALT CONCRETE MIX (PG76-22) GRADING "CM"

④ TACK COAT
ITEM 403-01 BITUMINOUS MATERIAL FOR TACK COAT (TC)
SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD

ALTERNATE AA1

③ TRACKLESS TACK COAT
ITEM 403-02.01 TRACKLESS TACK COAT (TC)
SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD

⑤ MINERAL AGGREGATE @ 2.00"± THICK FOR SHOULDERS
ITEM 303-01 MINERAL AGGREGATE, TYPE "A" BASE, GRADING "D"

⑥ COLD PLANING @ 1.50"± THICK (APPROX. 157.5 LBS./S.Y.)
ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT

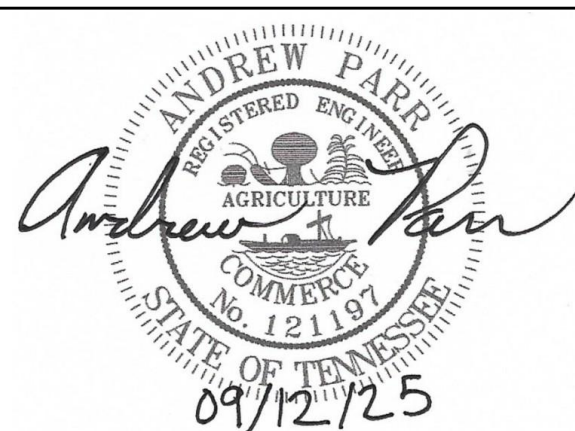
⑦ COLD PLANING @ 2.75"± THICK (APPROX. 288.75 LBS./S.Y.)
ITEM 415-01.01 COLD PLANING BITUMINOUS PAVEMENT

ALTERNATE AA2

③ TACK COAT (TC) (APPROX. 0.15 GAL./S.Y.)
ITEM 403-02.02 HOT APPLIED TACK COAT

SEE SHEET NO. 2B2 FOR RAMP INFORMATION

SEALED BY

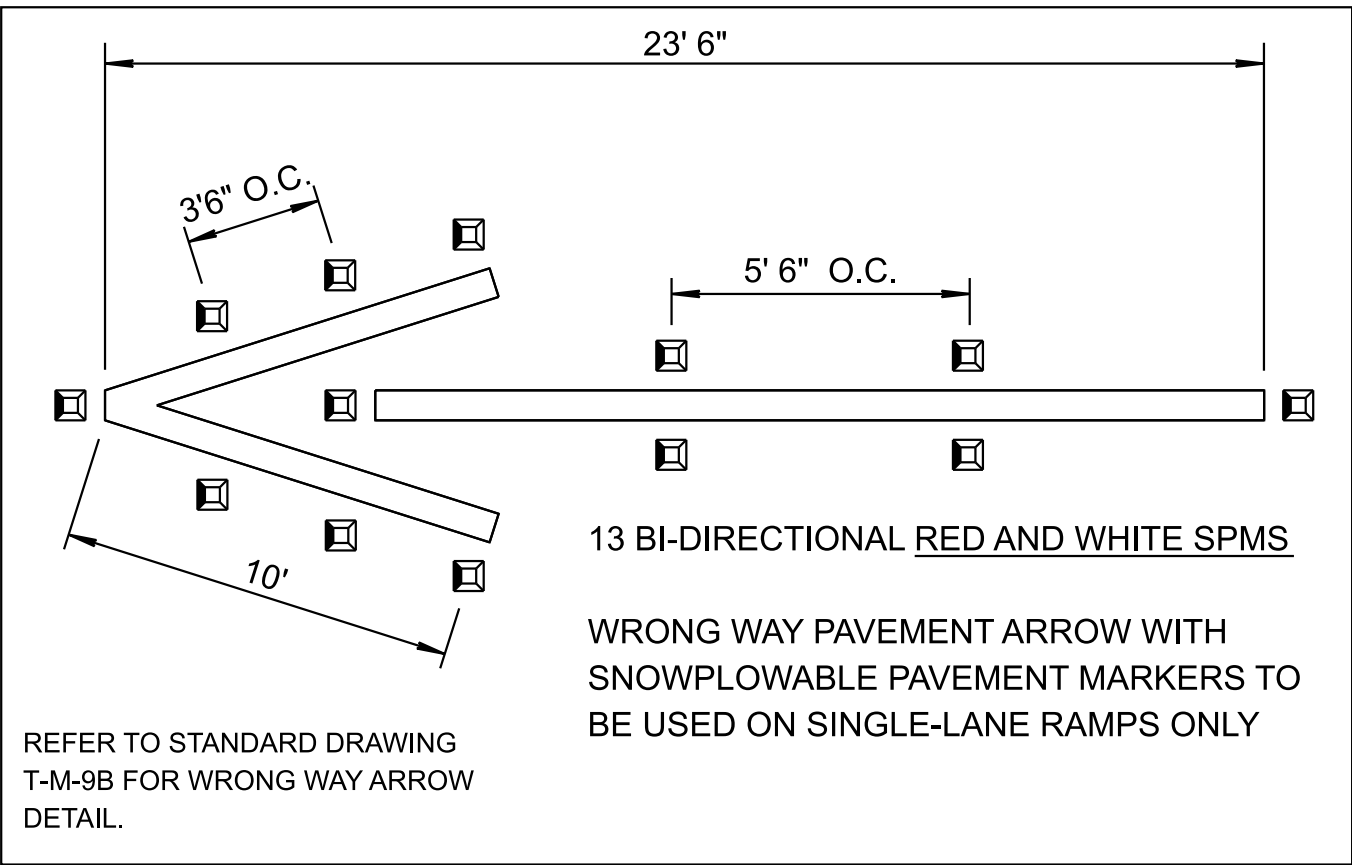


STATE OF TENNESSEE
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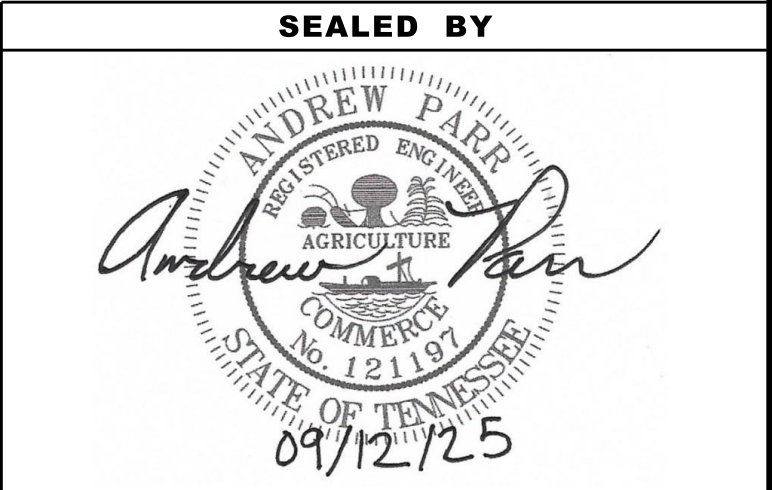
TYPICAL
SECTIONS AND
PAVEMENT
SCHEDULE

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-81-1(142)	2B2

I-81 INTERCHANGE	Sheet #	Length (ft)	Lane Width(s) (ft)	Outside Shoulder	Inside shoulder	Type of Typical
SR-36 / Fort Henry Dr. - Exit 59						
NB Ext. Ramp - Lane Taper	4	190'	0' - 12'	11'	N/A	Asphalt Typical 1
NB Ext. Ramp - Deceleration Lane	4	275'	12'	11'	N/A	Asphalt Typical 1
NB Ext. Ramp - Asphalt Ramp (Adjacent to Gore Area)	4	210'	12' - 14'	6'	N/A	Asphalt Typical 1
NB Ext. Ramp - Overlay Transition	4	125'	14'	6'	4'	Detail #2 on Sht. 2G
NB Ext. Ramp - Asphalt Ramp (Asphalt Shoulders)	4	140'	14'	6'	4'	Asphalt Typical 2
NB Ext. Ramp - Concrete Ramp (Asphalt Shoulders)	4/4A	685'	N/A	6'	4'	Concrete Typical 1
NB Ent. Ramp - Lane Taper	4B	560'	0' - 12'	11'	N/A	Asphalt Typical 1
NB Ent. Ramp - Acceleration Lane	4B	300'	12'	11'	N/A	Asphalt Typical 1
NB Ent. Ramp - Asphalt Ramp (Adjacent to Gore Area)	4B	165'	12' - 14'	6'	N/A	Asphalt Typical 1
NB Ent. Ramp - Overlay Transition	4A/4B	125'	14'	6'	4'	Detail #2 on Sht. 2G
NB Ent. Ramp - Asphalt Ramp (Asphalt Shoulders)	4A	35'	14'	6'	4'	Asphalt Typical 2
NB Ent. Ramp - Concrete Ramp (Asphalt Shoulders)	4A	770'	N/A	6'	4'	Concrete Typical 1
SB Ext. Ramp - Lane Taper	4B	258'	0' - 12'	11'	N/A	Asphalt Typical 1
SB Ext. Ramp - Asphalt Ramp (Adjacent to Gore Area)	4B	242'	12' - 14'	6'	N/A	Asphalt Typical 1
SB Ext. Ramp - Overlay Transition	4A	125'	14'	6'	4'	Detail #2 on Sht. 2G
SB Ext. Ramp - Asphalt Ramp (Asphalt Shoulders)	4A	130'	14'	6'	4'	Asphalt Typical 2
SB Ext. Ramp - Concrete Ramp (Asphalt Shoulders)	4A	583'	N/A	6'	4'	Concrete Typical 1
SB Ext. Ramp - Concrete Ramp (Asphalt Shoulders)	4A	117'	N/A	6'	N/A	Concrete Typical 2
SB Ext. Ramp - Concrete Ramp (Asphalt Shoulders)	4A	96'	N/A	N/A	4'	Concrete Typical 2
SB Ent. Ramp - Lane Taper	4	355'	0' - 12'	11'	N/A	Asphalt Typical 1
SB Ent. Ramp - Acceleration Lane	4	350'	12'	11'	N/A	Asphalt Typical 1
SB Ent. Ramp - Asphalt Ramp (Adjacent to Gore Area)	4	113'	12' - 14'	6'	N/A	Asphalt Typical 1
SB Ent. Ramp - Overlay Transition	4	125'	14'	6'	4'	Detail #2 on Sht. 2G
SB Ent. Ramp - Asphalt Ramp (Asphalt Shoulders)	4	105'	14'	6'	4'	Asphalt Typical 2
SB Ent. Ramp - Concrete Ramp (Asphalt Shoulders)	4/4A	480'	N/A	6'	4'	Concrete Typical 1
SB Ent. Ramp - Concrete Ramp (Asphalt Shoulders)	4A	195'	N/A	6'	N/A	Concrete Typical 2
SB Ent. Ramp - Concrete Ramp (Asphalt Shoulders)	4A	132'	N/A	N/A	4'	Concrete Typical 2



1 FOR N.B. EXIT RAMP NO. 59 TO SR-36 (L.M. 5.48)
1 FOR S.B. EXIT RAMP NO. 59 TO SR-36 (L.M. 5.91)



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL
SECTIONS

GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE "A" LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.
- (9) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 8" ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.03, ENHANCED FLATLINE THERMO PVMT MRKNG (8IN BARRIER LINE), L.F. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

- (19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

- (2) THE CONTRACTOR SHALL BE REQUIRED TO COLD PLANE AND PAVE IN THE DIRECTION OF TRAFFIC.
- (3) THE CONTRACTOR SHALL ATTACH A DEVICE TO THE SCREED OF THE PAVER SUCH THAT MATERIAL IS CONFINED AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A CONSOLIDATED WEDGE-SHAPE PAVEMENT EDGE OF APPROXIMATELY 25 TO 30 DEGREES AS IT LEAVES THE PAVER (MEASURED FROM A LINE PARALLEL TO THE PAVEMENT SURFACE.) THE DEVICE SHALL MEET THE REQUIREMENTS THAT ARE CURRENTLY SET FORTH IN SPECIAL PROVISION 407SE.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

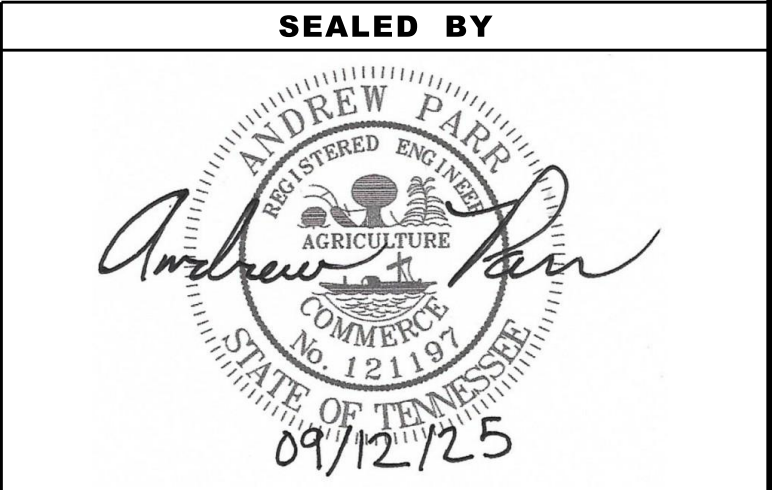
SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

TRAFFIC CONTROL DIRECTIONAL SIGNING

- (1) ON ALL ACCESS CONTROLLED AND INTERSTATE RECONSTRUCTION AND NEW CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL UTILIZE ALL EXISTING DIRECTIONAL SIGNING FOR AS LONG AS POSSIBLE. THESE EXISTING SIGNS CAN BE MOVED USING TEMPORARY SUPPORTS AS NEEDED. AS SOON AS THESE EXISTING DIRECTIONAL SIGNS COME DOWN PERMANENTLY, THE CONTRACTOR SHALL HAVE UP AT LEAST ONE NEW TEMPORARY “ADVANCE GUIDE SIGN” AND ONE NEW TEMPORARY “EXIT DIRECTIONAL SIGN” AT ALL EXIT RAMPS. THESE SIGNS ARE TO BE MAINTAINED WITHIN CLEAR VIEW OF THE PUBLIC ON THE RIGHT SIDE OF THE HIGHWAY AND SHALL BE REPLACED IF DAMAGED, DURING ALL PHASES OF CONSTRUCTION, AS DIRECTED BY THE ENGINEER.
- (2) THE SIZE OF THESE NEW TEMPORARY SIGNS WILL BE DETERMINED BY THE MESSAGE. THE MESSAGE SHALL BE THE SAME AS THE EXISTING SIGN THAT THESE NEW TEMPORARY SIGNS WILL BE REPLACING. THE LETTER SIZE SHALL BE A MINIMUM OF 8 INCH, “D” UPPER CASE LETTER. THE DIRECTIONAL ARROW WILL BE A “B” ARROW AT A 45 DEGREE ANGLE (SAME ANGLE AS THE EXISTING ARROW). THE MATERIAL SHALL BE 0.100 INCH SHEET ALUMINUM; THE COLOR SHALL BE A REFLECTIVE GREEN BACKGROUND WITH REFLECTIVE WHITE COPY.
- (3) ALL WORK AND MATERIAL TO MAKE THESE NEW TEMPORARY DIRECTIONAL SIGNS ALONG WITH ADEQUATE SUPPORTS AND TO MOVE THEM AS NEEDED DURING EACH PHASE OF CONSTRUCTION WILL BE PAID FOR UNDER ITEM NO. 712-06, AS DIRECTED BY THE ENGINEER.
- (4) SOME OF THESE DIRECTIONAL SIGNS WILL NEED AN INTERSTATE, U.S., OR A STATE HIGHWAY SHIELD, A CARDINAL DIRECTION, AND A DIRECTION ARROW TO ACCOMPANY THE DIRECTIONAL SIGN. THESE SIGNS SHALL BE MOUNTED BELOW THE DIRECTIONAL SIGN.
- (5) ALL EXISTING “EMERGENCY REFERENCE MARKERS” AND “HOSPITAL SIGNS” SHALL BE MAINTAINED WITHIN FULL VIEW OF THE MOTORING PUBLIC THROUGHOUT ALL PHASES OF CONSTRUCTION. ALL WORK IN MOVING AND TEMPORARY SUPPORTS SHALL BE PAID FOR UNDER ITEM NO. 712-06.
- (6) WHEN “LOGO” SIGNS ARE ON ACCESS CONTROLLED AND INTERSTATE RECONSTRUCTION AND NEW CONSTRUCTION PROJECTS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THESE SIGNS IN FULL VIEW TO THE MOTORING PUBLIC DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL BE HELD RESPONSIBLE TO THE DEPARTMENT FOR THE REIMBURSEMENT OF THE SIGN FACE IF IT IS DAMAGED. ALL WORK IN MOVING THESE “LOGO” SIGNS AND THE TEMPORARY SUPPORTS ARE TO BE PAID FOR UNDER ITEM NO. 712-06, AS DIRECTED BY THE ENGINEER. THE SUPPORTS FOR THE FINAL LOCATION OF THESE SIGNS WILL BE PAID FOR UNDER OTHER ITEMS OF CONSTRUCTION.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

GENERAL
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GENERAL NOTES (CONT'D)

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2)

IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3)

A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4)

TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5)

USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (6)

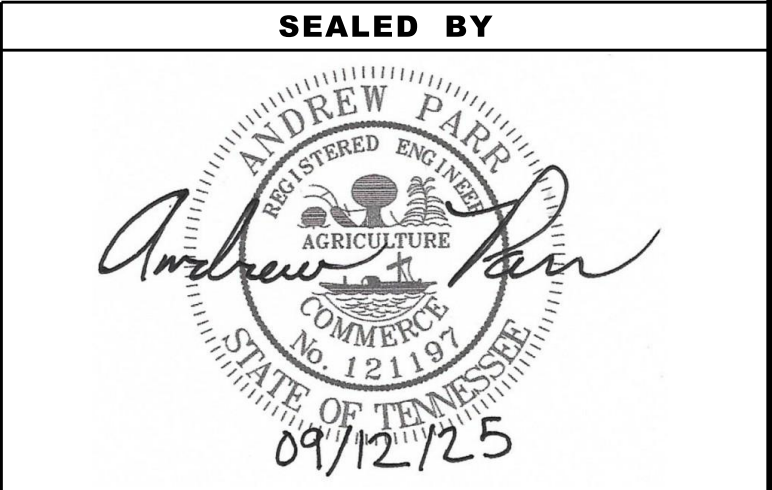
THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.
- (7)

ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (8)

ALL DETOURS SHALL BE PAVED, STRIPED, SIGNED, AND FLEXIBLE DRUMS ARE TO BE IN PLACE BEFORE IT IS OPENED TO TRAFFIC.
- (9)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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STATE OF TENNESSEE
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GENERAL
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SPECIAL NOTES

PAVEMENT

RESURFACING

- (1)

TRAFFIC WILL BE ALLOWED TO TEMPORARILY DRIVE ON THE MILLED SURFACE OF THE ROADWAY UNDER THE FOLLOWING CONDITIONS ONLY:

A.

THE MILLED SURFACE IS FINE TEXTURED. THE FINE TEXTURE SHALL BE OBTAINED BY A MILLING MACHINE UTILIZING A MILLING HEAD WITH TEETH SPACING 3/8" OR LESS OPERATING AT LESS THAN 80 FEET PER MINUTE.

B.

THE SURFACE SHALL BE SWEEPED AND CLEANED OF ALL LOOSE MATERIALS.

C.

THE MILLED SURFACE SHALL BE PAVED WITHIN 72 HOURS IF THE CURRENT ADT IS ≥ 70,000 OR WITHIN 96 HOURS IF THE CURRENT ADT IS < 70,000.

D.

RAIN OR INCLEMENT WEATHER IS NOT EXPECTED OR FORECASTED WITHIN 48 HOURS AFTER MILLING.

E.

ALL APPLICABLE SIGNING IS INSTALLED IN ACCORDANCE WITH THE MUTCD. SIGNING SHALL INCLUDE MOTORCYCLE WARNING SIGNS (W8-15P) PLACED IN ADVANCE OF ANY MILLED AREAS

F.

IF MILLED SURFACE BEGINS TO DETERIORATE, PAVING TO COVER UP DETERIORATING MILLED SURFACES SHOULD OCCUR AS DIRECTED BY THE ENGINEER DURING THE NEXT WORKING DAY. IF SEVERE DISTRESS OCCURS, IMMEDIATE RESPONSE WILL BE REQUIRED.

G.

ONLY ONE LANE IN EACH DIRECTION SHALL HAVE A MILLED SURFACE AT ONE TIME.
- (2)

THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

A.

BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.

B.

ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

C.

ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.
- (3)

SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (4)

THE INSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE INSIDE TRAFFIC LANE.
- (5)

THE CONTRACTOR SHALL TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK SO AS NOT TO DAMAGE THE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS). IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS TO ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.
- (6)

AT THE START OF EACH NIGHT’S PAVING PRODUCE APPROXIMATELY 15 TONS OF THE BITUMINOUS PAVEMENT FOR THE PURPOSE OF PLANT START UP AND HEATING UP THE MATERIAL TRANSFER DEVICE (MTD). THIS MATERIAL SHALL BE UNLOADED INTO THE MTD AT A MINIMUM TEMPERATURE OF 280F AND THE ENTIRE LOAD DISCHARGED THROUGH THE MTD AND WASTED OFF THE PROJECT SITE IMMEDIATELY PRIOR TO THE COMMENCING PAVING OPERATIONS.

PAVEMENT MARKING

- (1)

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20.
- (2)

UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

- (1)

IF THE CONTRACTOR ELECTS TO UTILIZE SIGNPOST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

- (1)

ITEM 303-01 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1)

THE DEPARTMENT SHALL RESERVE THE RIGHT TO REOPEN LANES AS NECESSARY WHEN TRAFFIC CONDITIONS ARE DEEMED UNACCEPTABLE (EXCESSIVE QUEUE LENGTHS AND DELAY TIMES). THE CONTRACTOR SHALL BE REQUIRED TO FULLY COOPERATE WITH THE PROJECT SUPERVISOR WHEN REQUESTED TO MAKE CHANGES TO THE TRAFFIC CONTROL.
- (2)

MESSAGE BOARDS SHALL BE USED NEAR INTERCHANGES AND/OR OTHER DESIGNATED AREAS IN ADVANCE OF THE WORK ZONE TO ALERT MOTORISTS OF POSSIBLE DELAYS AND RECOMMEND THE USE OF ALTERNATE ROUTES. THE MESSAGES SHALL BE UPDATED AS OFTEN AS NECESSARY SO THAT THE MOTORISTS OBTAIN CURRENT TRAFFIC INFORMATION. MESSAGE BOARDS SHALL BE RELOCATED AS NECESSARY AS THE WORK PROGRESSES. THE CONTRACTOR SHALL BE REQUIRED TO IDENTIFY AN INDIVIDUAL WORKING ON THE PROJECT THAT WILL BE RESPONSIBLE FOR KEEPING THE MESSAGES CURRENT AND RELOCATING MESSAGE BOARDS AS REQUESTED BY TDOT. THE PROJECT SUPERVISOR SHALL HAVE THE AUTHORITY TO APPROVE ALL MESSAGES AND REQUIRED CHANGES AT ANY TIME DUE TO CHANGING TRAFFIC CONDITIONS.

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09/12/25

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SPECIAL
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SPECIAL NOTES

CSXT SAFETY NOTES:

- (1)

COMPLY WITH THE CONSTRUCTION SUBMISSION CRITERIA OF THE CSXT PUBLIC INFORMATION DOCUMENT AND CONSTRUCTION REQUIREMENTS OF THE PUBLIC PROJECTS MANUAL, WHICH IS AVAILABLE AT THE FOLLOWING URL: <https://www.csx.com/index.cfm/about-us/property/>
- (2)

ALL WORK IN THE FRA RED ZONE (WITHIN 4 FEET FROM OUTSIDE OF THE RAIL ON EACH SIDE OF THE TRACK) WILL BE ALLOWED ONLY WITH A CSXT, FRA QUALIFIED FLAGMAN OR WATCHMAN AS SPECIFIED BY THE LOCAL ENGINEERING REPRESENTATIVE.
- (3)

ALL WORK BEYOND 4 FEET FROM THE OUTSIDE RAILS AND WITHIN 25 FEET MUST BE DONE UNDER THE SUPERVISION OF A QUALIFIED INSPECTOR OR CSXT FLAGMAN.
- (4)

ALL ACTIVITIES WITHIN 25 FEET FROM THE CENTERLINE OF THE NEAREST TRACK ARE CONSIDERED FOULING THE TRACK. DURING CONSTRUCTION, CLEAR THE FOUL AREA BY REMOVING ALL PERSONNEL AND EQUIPMENT A MINIMUM OF 25 FEET FROM THE CENTERLINE OF THE NEAREST TRACK DURING TRAIN OPERATIONS. NOTICE FOR CLEARING THE FOULED AREA DURING TRAIN OPERATIONS WILL BE GIVEN BY AN ONSITE RAILROAD PROTECTION FLAGMAN. CLEAR THE FOUL AREA AND REMAIN CLEAR OF THE FOULED AREA UNTIL ALLOWED TO RETURN AS INSTRUCTED BY THE FLAGMAN. THIS REQUIREMENT TO CLEAR AND REMAIN CLEAR OF THE FOULED AREA MAY OCCUR AT ANY TIME DURING THE CONSTRUCTION.
- (5)

CERTAIN TYPES OF WORK DONE BEYOND 25 FEET FROM THE OUTSIDE OF THE RAILS, AND WITH EQUIPMENT THAT WILL NOT REACH BEYOND THIS POINT, MAY BE DONE WITHOUT FLAGGING PROTECTION OR A WATCHMAN. THIS MUST BE APPROVED BY THE LOCAL ENGINEERING REPRESENTATIVE, THE AREA MUST BE PROTECTED BY A CONSTRUCTION FENCE, AND THE WORK MUST BE STATIONARY
- (6)

ALL WORKERS WILL REMAIN OFF THE TRACKS. IF NECESSARY TO PERFORM THE WORK ON TRACK, PROTECTION WILL BE PROVIDED AS STATED ABOVE.
- (7)

ALL WORKERS MUST COMPLY WITH FEDERAL, STATE, AND LOCAL LAWS AND REGULATIONS, INCLUDING BUT NOT LIMITED TO THOSE OF THE OCCUPATIONAL SAFETY AND HEALTH ADMINISTRATION (OSHA) AND THE FEDERAL RAILROAD ADMINISTRATION (FRA).
- (8)

AT LEAST THIRTY (30) DAYS ADVANCED NOTIFICATION MUST BE GIVEN TO THE RAILROAD REPRESENTATIVE, TO SCHEDULE A RAILROAD FLAGMAN.
- (9)

THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY, FOR STORAGE OF MATERIALS OR EQUIPMENT, WITHOUT PRIOR WRITTEN APPROVAL FROM CSXT.
- (10)

THE CONTRACTOR SHALL CONDUCT ITS WORK AT ALL TIMES, IN A MANNER WHICH WILL PROTECT CSXT’S PROPERTY AND TRACK FACILITIES FROM DAMAGE AND WITHOUT INTERRUPTION TO TRAIN OPERATIONS
- (11)

PRIOR TO THE INSTALLATION OF ANY SIGNAGE WITHIN CSXT RIGHT-OF-WAY, CONTRACTORS MUST CONTACT THE RAILROAD’S REPRESENTATIVE FOR LOCATION OF ALL UNDERGROUND SIGNAL UTILITIES.
- (12)

ANY VIOLATION OF ANY CSXT RULES, REGULATIONS OR POLICIES, MAY RESULT IN REMOVAL OF CONTRACTOR PERSONNEL FROM THE RIGHT-OF-WAY.
- (13)

NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO SET UP TO WORK OR PARK WITHIN BOOM DISTANCE PLUS 15 FEET OF THE CENTERLINE OF TRACK WITHOUT SPECIFIC PERMISSION FROM THE RAILROAD. NO CRANE OR BOOM EQUIPMENT SHALL BE ALLOWED TO FOUL TRACK, WORK WITHIN THE FOUL ZONE, OR LIFT A LOAD OVER THE TRACK WITHOUT FLAGGING PROTECTION AND PERMISSION FOR TRACK TIME FROM THE RAILROAD.
- (14)

ALL WORKMEN AND MACHINE OPERATORS SHALL STAY WITH THEIR MACHINES WHEN CRANE OR BOOM EQUIPMENT IS POINTED TOWARD THE TRACK. ALL CRANES AND BOOM EQUIPMENT SHALL STOP WORK AND CLEAR TRACK WHILE TRAIN IS PASSING. SWINGING LOADS SHALL BE SECURED TO PREVENT MOVEMENT WHILE TRAIN IS PASSING AND NO LOADS SHALL BE SUSPENDED ABOVE A MOVING TRAIN. ALL CRANES AND BOOM EQUIPMENT SHALL BE TURNED AWAY FROM THE TRACK AFTER EACH WORKDAY OR WHENEVER UNATTENDED BY AN OPERATOR.
- (15)

ALL WORK MUST BE STOPPED WHILE TRAINS ARE PASSING WITHIN THE WORK ZONE.
- (16)

“ONE CALL” SERVICES DO NOT LOCATE BURIED RAILROAD SIGNAL AND COMMUNICATIONS LINES. THE CONTRACTOR SHALL CONTACT THE RAILROAD’S REPRESENTATIVE FIVE (5) DAYS IN ADVANCE OF THOSE PLACES WHERE EXCAVATION, PILE DRIVING, OR HEAVY LOADS MAY DAMAGE RAILROAD UNDERGROUND LINES ON RAILROAD PROPERTY. UPON REQUEST FROM THE CONTRACTOR OR AGENCY, RAILROAD SIGNAL FORCES WILL LOCATE AND PAINT MARK OR FLAG RAILROAD UNDERGROUND SIGNAL, COMMUNICATION, AND POWER LINES IN THE AREA TO BE DISTURBED FOR THE CONTRACTOR. THE CONTRACTOR SHALL AVOID EXCAVATION OR OTHER DISTURBANCE OF THESE LINES WHICH ARE CRITICAL TO THE SAFETY OF THE RAILROAD AND THE PUBLIC. IF DISTURBANCE OR EXCAVATION IS REQUIRED NEAR A BURIED RAILROAD SIGNAL, COMMUNICATION, OR POWER LINE, THE LINE SHALL BE POTHOLED MANUALLY WITH CAREFUL HAND EXCAVATION BY THE CONTRACTOR AND PROTECTED BY THE CONTRACTOR DURING THE COURSE OF THE DISTURBANCE UNDER THE SUPERVISION AND DIRECTION OF A RAILROAD SIGNAL REPRESENTATIVE.
- (17)

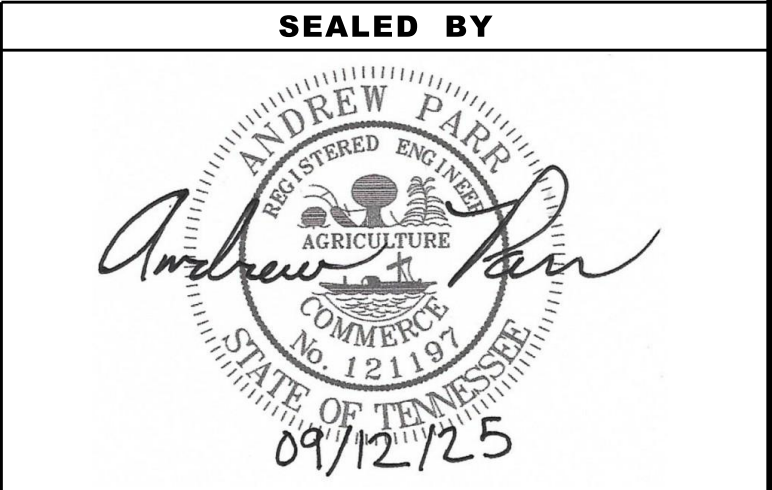
ALL SOILS EXCAVATED WITHIN CSXT’S RAILROAD RIGHT-OF-WAY SHALL REMAIN ON CSXT’S RIGHT-OF-WAY. TESTING OF SOILS ON CSXT ROW IS PROHIBITED WITHOUT PRIOR WRITTEN CSXT AUTHORIZATION. ANY SOILS EXCAVATED ON CSXT ROW CAN BE REUSED ON THE ROW PROVIDED PLACING SOILS ALONG CSXT ROW POSES NO ADVERSE IMPACTS TO THE EXISTING TERRAIN, DRAINAGE OR ENVIRONMENT. SHOULD SOIL NEED TO BE REMOVED FROM CSXT ROW, THE CSXT ENVIRONMENTAL DEPARTMENT WILL SAMPLE THE SOIL FOR DISPOSITION. SOIL STAGED ON CSXT MUST FOLLOW CSXT PROTOCOL AND BE PROPERLY STORED AND/OR PROTECTED FROM THE ELEMENTS AND POTENTIAL EXPOSURE.
- (18)

CONFORM TO CSX GUIDELINES FOR TEMPORARY SHORING.
- (19)

THE CONTRACTOR SHALL NOTIFY AND COORDINATE THEIR WORK WITH THE FOLLOWING CSXT REPRESENTATIVE:

CROUCH ENGINEERING, INC.5115 MARYLAND WAY, SUITE 225
BRENTWOOD, TN 37027
ATTN: MR. SCOTT VICK, P.E.
PHONE: 615-791-0630
EMAIL: SVICK@CROUCHENGINEERING.COM

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-81-1(142)	2D1



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SPECIAL
NOTES

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ROADWAY DESIGN DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL

- (20) EXCEPT AS OTHERWISE SPECIFIED, THERE ARE NO KNOWN SPECIAL ENVIRONMENTAL FACTORS PRESENT ON THIS PROJECT THAT INDICATE A NEED FOR SEASONAL LIMITATIONS ON THE CLEARING, GRUBBING, EXCAVATION, GRADING, CUTTING OR FILLING OPERATIONS OR ON THE TOTAL AREA OF EXPOSED SOIL.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL FIELD REVIEW MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

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RESURF	2025	NH-I-81-1(142)	2E

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09/12/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL
NOTES

EROSION PREVENTION AND SEDIMENT CONTROL NOTES

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

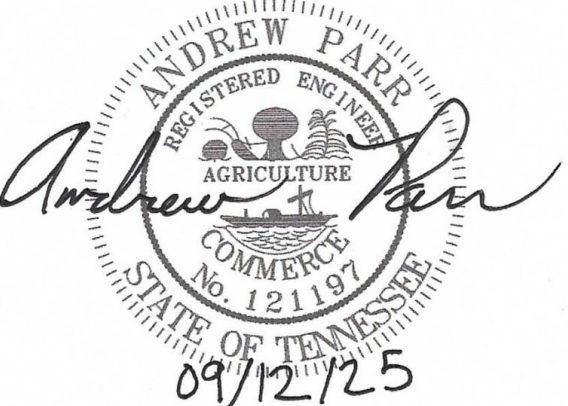
SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

EROSION PREVENTION
AND SEDIMENT
CONTROL NOTES

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	NH-I-81-1(142)	2F

TRAFFIC CONTROL SIGN TABULATION (RESURFACING)					
M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES L x W	S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
E5-1	EXIT	18" x 24"	3	2	6
G20-1	ROAD WORK NEXT 6 MILES	48" x 24"	8	4	32
G20-2	END ROAD WORK	48" x 24"	8	6	48
R1-2	YIELD	60" x 60" x 60"	11	2	22
R2-1	SPEED LIMIT	48" x 60"	20	2	40
R4-1(MOD)	DO NOT PASS IN RIGHT LANE	120" x 42"	35	2	70
TN-44	WORKERS PRESENT REDUCE SPEED	78" x 60"	33	4	130
TN-64	MOTORCYLCE WARNING SIGN	24" x 18"	3	4	12
W1-4CR	RIGHT SHIFT ARROWS	48" x 48"	16	4	64
W1-4CL	LEFT SHIFT ARROWS	48" x 48"	16	4	64
W1-4AR	RIGHT SHIFT ARROW	48" x 48"	16	4	64
W3-2	YIELD AHEAD	36" x 36"	9	2	18
W3-4	BE PREPARED TO STOP	36" x 36"	9	4	36
W4-2L	LANE REDUCTION	48" x 48"	16	4	64
W4-2R	LANE REDUCTION	48" x 48"	16	4	64
W8-9A	SHOULDER DROP OFF	48" x 48"	16	14	224
W8-11	UNEVEN LANES	48" x 48"	16	14	224
W8-15	GROOVED PAVEMENT	48" x 48"	16	4	64
W20-1	ADVANCE ROAD WORK	48" x 48"	16	14	224
W20-5L	LEFT LANE CLOSED AHEAD	48" x 48"	16	4	64
W20-5R	RIGHT LANE CLOSED AHEAD	48" x 48"	16	4	64
W21-5L	RIGHT SHOULDER CLOSED AHEAD	48" x 48"	16	4	64
W21-5R	RIGHT SHOULDER CLOSED AHEAD	48" x 48"	16	4	64
SPECIAL	MERGE NOW	48" x 48"	16	2	32
SUPP.	SUPP. SIGN NEXT 6 MILES	36" x 18"	5	2	9
				TOTAL	1767

NOTE: THE CONSTRUCTION SIGNING IS TO BE A MINIMUM. OTHER SIGNS MAY BE REQUIRED AS DIRECTED BY THE TDOT MANAGER

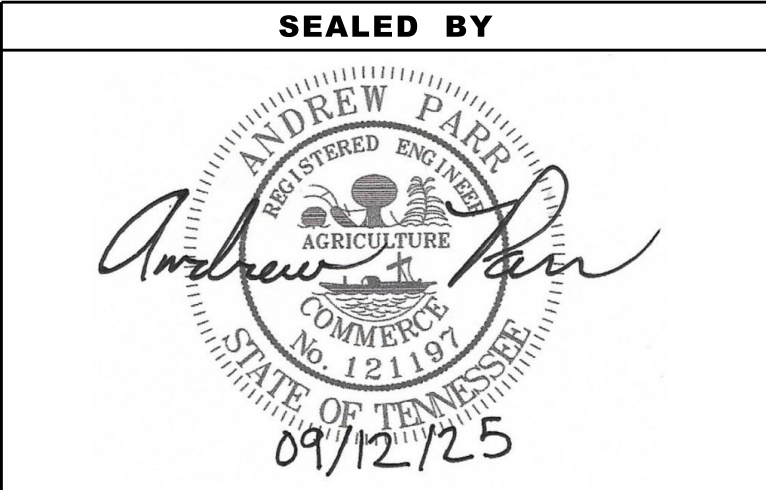
PAVEMENT QUANTITIES								
LOCATION (ROADWAY)	TYPE - GRADE - PAY ITEM (TON)							COLD PLANING BITUMINOUS PAVEMENT (TON)
	MINERAL AGG.	BITUMINOUS PLANT MIX BASE (HOT MIX)	TACK COAT			ASPHALTIC CONCRETE SURFACE (HOT MIX)		
	D	CM				D	OGFC	
	303-01	307-03.12	403-02.01	403-02.02	403-01	411-03.10	411-03.23	
MAINLINE		10636.0	206.0	155.0	69.0		10832.0	14773.0
RAMPS	269.0	592.0	18.0	14.0	4.0		1215.0	889.0
GORE AREAS		103.0	2.0	1.0	1.0		73.0	98.0
BRIDGES						3600.0		
PLANT START UP							230.0	
TOTALS	269.0	11331.0	226.0	170.0	74.0	3600.0	12350.0	15760.0

GUARDRAIL TABULATION				
LOG MILE	GUARDRAIL			
	GUARDRAIL TRANSITION	GUARDRAIL REMOVED	EARTH PAD TYPE 38 GR END TREATMENT	TYPE 38 MASH TL3
	27IN TO 31IN 705-02.10 EACH	706-01 (L.F.)	705-04.09 EACH	705-06.20 (EACH)
NB 4.863 RT	1	47*		1
SB 5.736 LT	1	47*	1	1
SB 5.738 RT	1	47*		1
SB 7.782 LT	1	47*		1
SB 8.088 LT	1	47*		1
SB 8.133 RT	1	47*		1
TOTALS	6	282*	1	6

* GUARDRAIL SHALL BE REMOVED SUCH THAT NEW COMBINED GUARDRAIL AND END TERMINAL LENGTH IS EQUAL TO OR GREATER THAN EXISTING.

BRIDGE DECK RECOMMENDATIONS (RESURFACING)				
BRIDGE NUMBER	LOCATION LOG MILE	CROSSES OVER/UNDER	BRIDGE LENGTH	BRIDGE DECK RECOMMENDATIONS
82100810011	5.03	LEBANON DRIVE	146'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810012	5.03	LEBANON DRIVE	146'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810013	5.41	CSX RAILROAD	174'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810014	5.41	CSX RAILROAD	174	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810015	5.65	SR-36	243'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810016	5.65	SR-36	243'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810017	6.53	SOUTH FORK HOLSTON RIVER	1094'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810018	6.54	SOUTH FORK HOLSTON RIVER	1024'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810019	7.98	BUTTERMILK ROAD	140'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT
82100810020	7.98	BUTTERMILK ROAD	168'	COLD PLANE 1.25"OF THE EXISTING ASPHALT AND REPLACE WITH 1.25" OF NEW D MIX ASPHALT

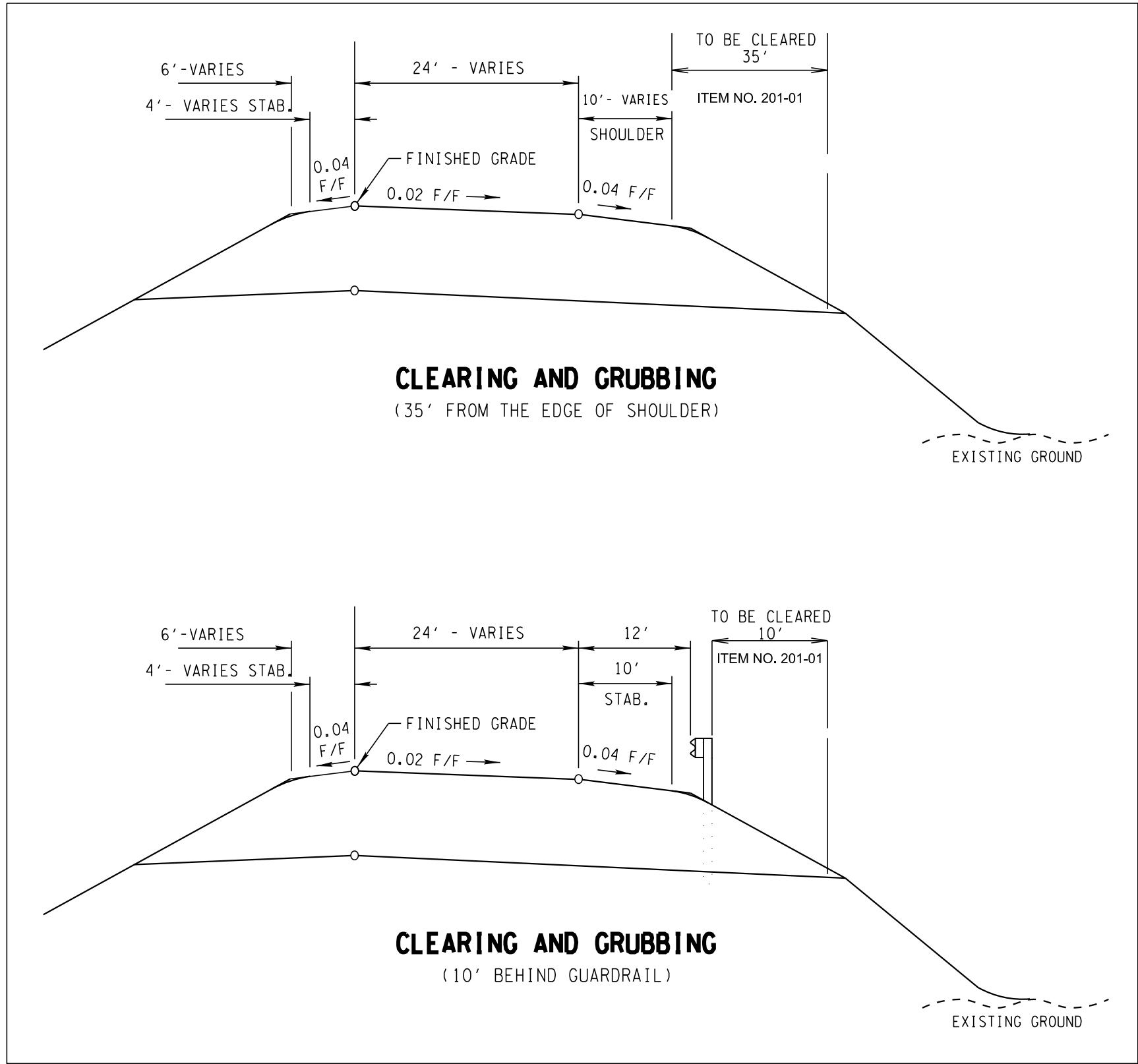
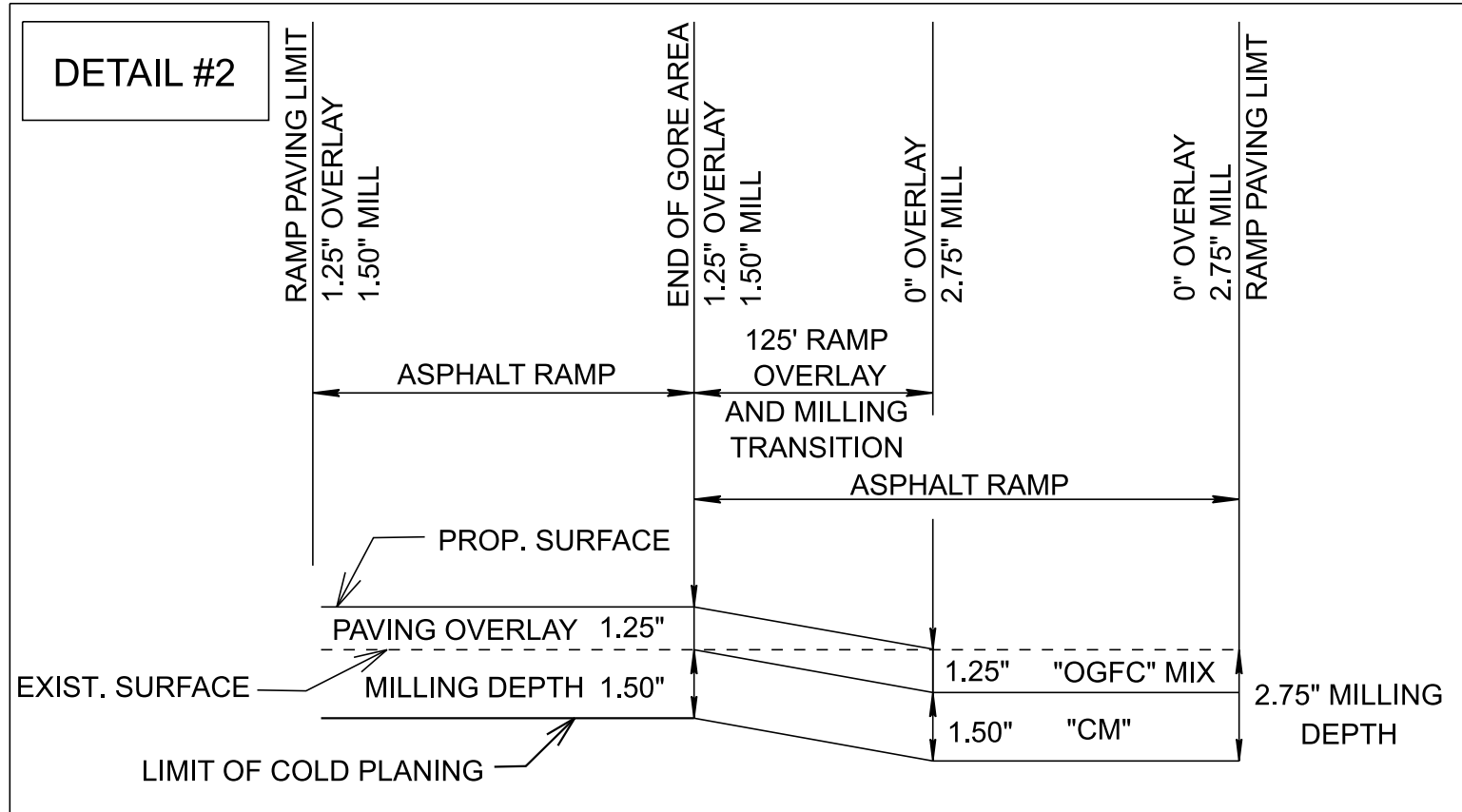
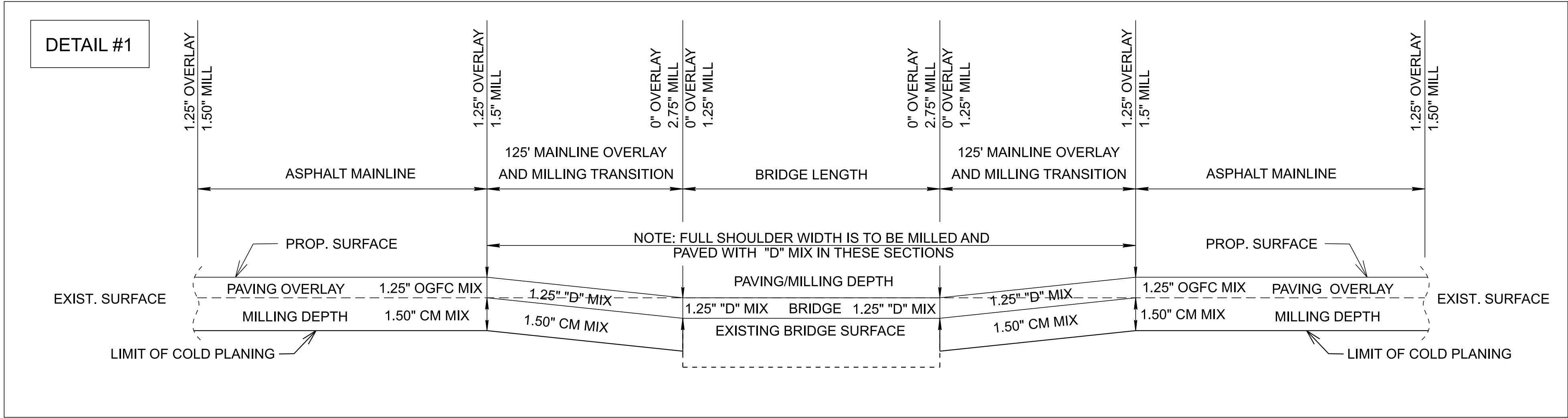
SPECIAL NOTE:
THE CONTRACTOR SHOULD TAKE EXTREME CARE WHEN COLD PLANING THE EXISTING ASPHALT OFF BRIDGE DECK, SO AS NOT TO DAMAGETHE EXISTING DECK SEALANT AND/OR EXPANSION JOINT MEMBERS (STEEL PLATES, BARS, AND/OR HEADERS).
IF DAMAGE DOES OCCUR, THE CONTRACTOR SHALL BE RESPONSIBLE FOR NECESSARY REPAIRS ON ALL DAMAGED MEMBERS TO THE SATISFACTION OF THE PROJECT ENGINEER AT NO ADDITIONAL COST.



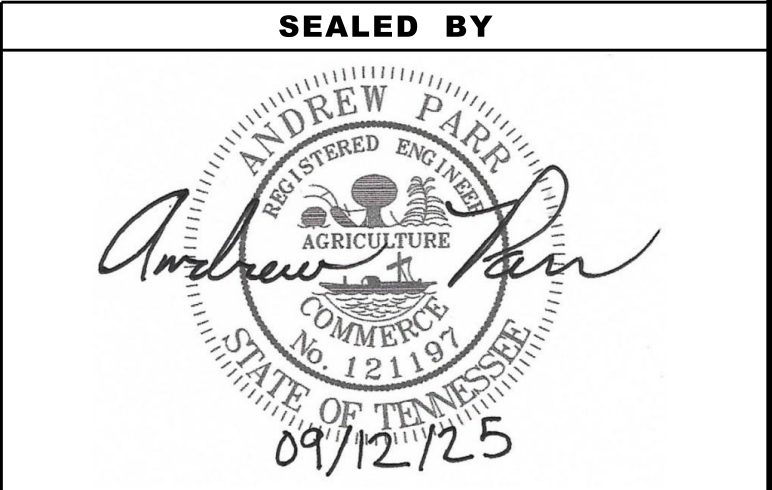
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TABULATED
QUANTITIES

TYPE	YEAR	PROJECT NO.	SHEET NO.
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THE CONTRACTOR WILL BE REQUIRED TO CUT TREES AND VEGETATION FLUSH WITH THE GROUND WITHIN 35' OF EDGE OF PAVEMENT OR WITHIN 10' IF PROTECTED BEHIND GUARDRAIL. NO ORNAMENTAL AND/OR DECORATIVE TREES SHALL BE CUT OUTSIDE OF THE CLEAR ZONE OR AS DIRECTED BY THE TDOT ENGINEER. ALL TREES AND VEGETATION THAT ARE CUT WILL BE DISPOSED OF AT A SITE APPROVED BY THE TDOT ENGINEER. COST TO BE INCLUDED IN THE UNIT BID PRICE FOR CLEARING AND GRUBBING, ITEM NO. 201-01.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

DETAIL SHEET

UTILITY NOTES

- (2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

CABLE:
CHARTER
10417 WALLACE ALLEY STREET
KINGSPORT, TN 37663
CONTACT: JEREMY SMITH
OFFICE PHONE: 423 212 2367
CELL PHONE: 423 341 0494
Email: JEREMY.SMITH@CHARTER.COM

ELECTRIC:
BRISTOL TENNESSEE ESSENTIAL SERVICES
2470 VOLUNTEER PARKWAY
BRISTOL, TN 37620
CONTACT: DAVID HACKER
OFFICE PHONE: 423 793 5548
CELL PHONE: 423 575 5548
Email: DHACKER@BTES.NET

GAS:
ATMOS
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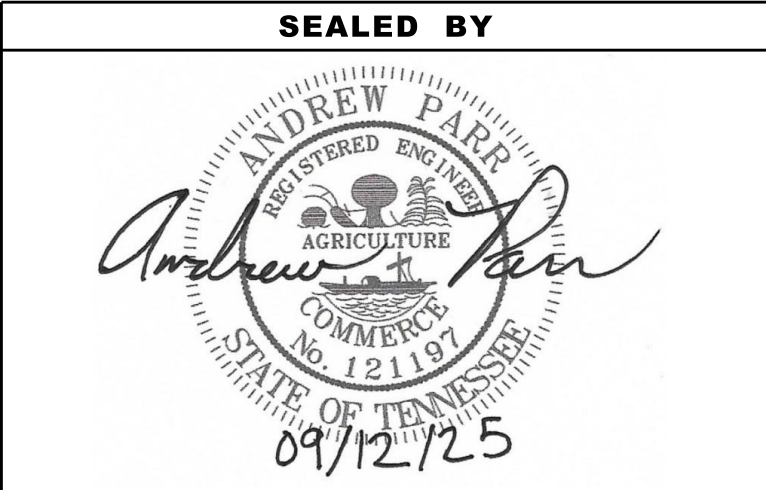
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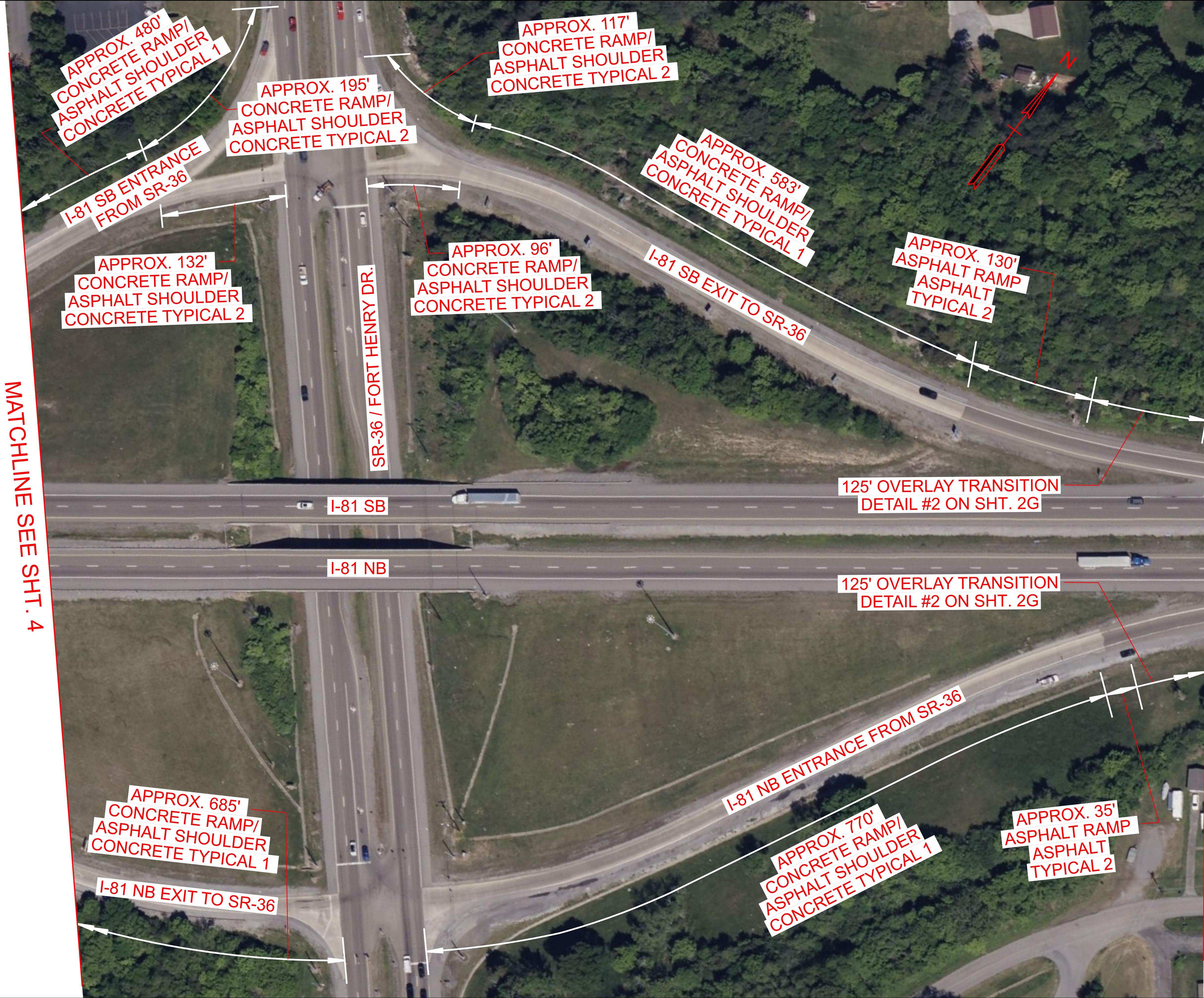
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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

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MATCHLINE SEE SHT. 4

MATCHLINE SEE SHT. 4B

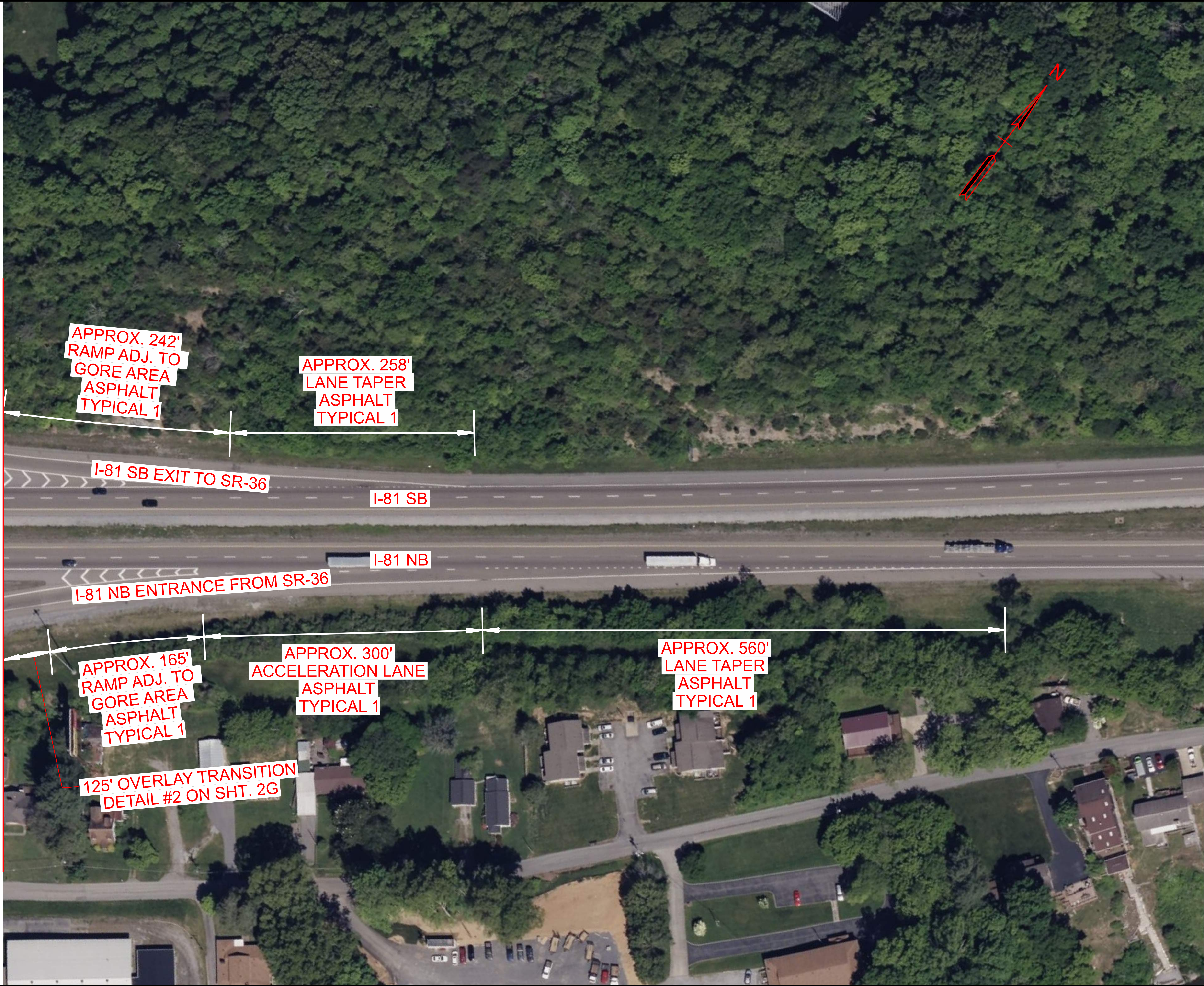
SEALED BY

ANDREW PARR
REGISTERED ENGINEER
AGRICULTURE
STATE OF TENNESSEE
NO. 12119
09/12/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RAMP
DETAILS

SCALE: 1" = 50'



MATCHLINE SEE SHT. 4A

TYPE	YEAR	PROJECT NO.	SHEET NO.
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SEALED BY



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

RAMP
DETAILS

SCALE: 1" = 50'

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:
- a.

WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- b.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- c.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.
- d.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.
2.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.
- a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- b.

IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- c.

WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3.

DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:
- a.

THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

- b.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.
- c.

THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.
- d.

THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4.

FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 0.75 INCH AND NOT EXCEEDING 2 INCHES.
- a.

WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
2.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:
- a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
3.

IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:
- a.

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:
- (1)

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
- (2)

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.
- b.

ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

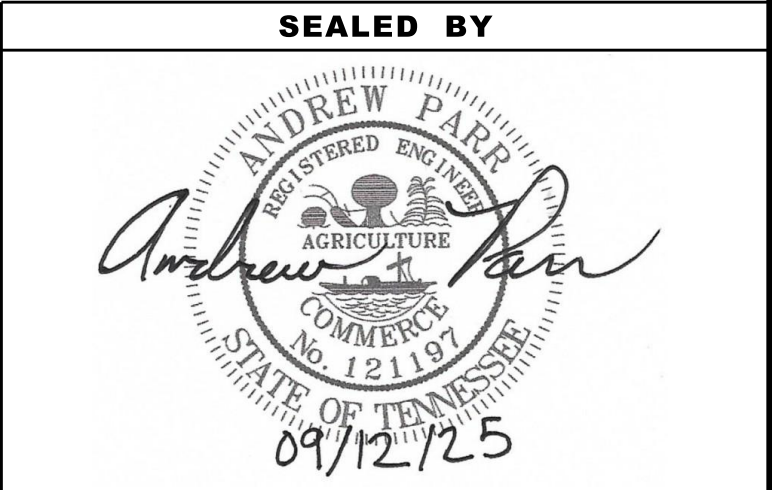
1.

WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.
2.

WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

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PAVEMENT EDGE
DROP-OFF NOTES
FOR
TRAFFIC CONTROL